

Foreword

On behalf of the Office of Global Maritime Situational Awareness and our partner agencies and organizations, I am pleased to present this summary report of the second annual Global Maritime Information Sharing Symposium to the global maritime community of interest.

Making this symposium a success involved the work, resources, and expertise of several organizations. Once again, I would like to thank the Maritime Administration, the Department of Justice Office of Community Oriented Policing Services, the staff of U.S. Coast Guard Headquarters, the Office of the Director of National Intelligence, the National Maritime Intelligence Center, U.S. Fleet Forces Command, Naval Cooperation and Guidance for Shipping, the Naval Postgraduate School, and the National Defense University, without whose involvement the success of GMISS 2009 would have been diminished.

Equally important to the outcome of GMISS 2009 were the more than 40 speakers, presenters and panelists. A shared understanding of the broad range of perspectives represented within the maritime community enabled participants to reach general consensus on a number of complex and controversial issues within the time limits of the symposium. I am grateful to our speakers for their knowledge and experience, and for their courage in speaking plainly about topics that can at times generate some very heated discussion.

Most importantly, the organizers of GMISS want to thank each participant who took time away from his or her primary concerns to spend three days helping us move several critical issues forward. The true meaning of your decision to come and contribute so much of your time within this difficult economic climate is genuinely understood and appreciated.

That said, I ask you to help us bring to resolution the issues raised at GMISS by joining us in our ongoing working groups. Much work remains to be accomplished. The maritime awareness policy decisions the government makes impact both the maritime industry and the global supply chain for which it is the lynchpin. Your knowledge and expertise are necessary to ensure these decisions capitalize on the full measure of the industry's potential contribution to maritime awareness, and are informed by a reasonable understanding of this incredibly complex system of systems.

Thank you again, and we look forward to seeing you at GMISS 2010 next summer.

Gary Seffel

Director
Office of Global Maritime Situational Awareness



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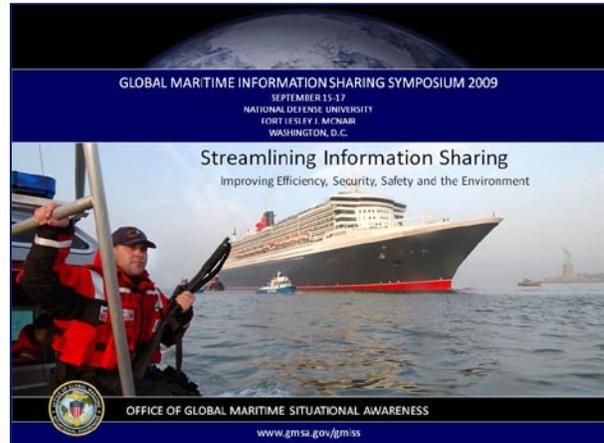
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I. Overview

The second annual Global Maritime Information Sharing Symposium (GMISS) was held September 15th through 17th, 2009 at the National Defense University on Fort Lesley J. McNair, Washington, D.C. The symposium brought together a diverse mix of more than 230 participants from maritime companies, associations, operations and fusion centers, navies, coast guards and government agencies from around the world.

GMISS is sponsored in conjunction with the U.S. Maritime Administration (MARAD) and under the auspices of the U.S. National Maritime Domain Awareness (MDA) Stakeholder Board which report to the Maritime Security Interagency Policy Committee of National Security Council. It is dedicated to enhancing the information sharing relationships between the maritime industry and government through the coordination of the Office of Global Maritime Situational Awareness (OGMSA).



Background =

A Need for New Approaches

OGMSA was created based on the understanding that there are significant benefits to be gained through an increase in the level of open exchange of non-classified maritime information (*please see Appendix A for more information on OGMSA*). As witnessed within other industries (airline, trucking, manufacturing, etc.), an increase in the open exchange of maritime information can improve the maritime industry's efficiency, together with its safety and security, and the environment within which it operates.

Within each of the United States' guiding documents and initiatives calling for greater global maritime security, such as the National Strategy for Maritime Security and the "Cooperative Strategy for 21st Century Sea Power," are such core concepts of openness as maximizing the share-ability of maritime data, multilateral versus bi-lateral data sharing, growing both long term interagency and international partnership trust, and collaborating with the maritime industry. When such concepts are acted upon while embracing a broader rationale for information sharing, such as commercial efficiency, maritime safety, and environmental management, the result is greater transparency in maritime affairs to the benefit of all. By sharing maritime data that benefits a broader range of potential contributors, a more complete picture of the entire maritime domain becomes available to support maritime security.

Key to this shift to openness is greater cooperation with the industry that operates throughout the domain, and a broadening of the dialog along non-traditional lines of communication and engagement.

Need for Conference Coordination

The realization of this fact spawned a surge in energy to expand industry outreach. However, it needed focus. By informal count, there were well over 20 commercial outreach meetings sponsored by U.S. government entities in Fiscal Year 2008. The plethora of conferences and symposiums, all purporting to enhance communications and information sharing between government agencies and the maritime industry, created the following list of complaints from industry and government:

- Too many conferences that say the same thing; objectives are the same.
- Too many conferences with no follow through.
- Too many one-way conversations (government talks, industry listens).
- Too many conferences that establish working groups but no usable products are created.
- Conference results are not shared across agencies, nor is there an attempt to build on issues raised.
- Too many duplicative information requests by the U.S. government and international organizations.
- Too many unilateral demands on the industry.



The U.S. Coast Guard Cutter USCGC Thetis (WMEC 910) sails in formation with maritime forces from Argentina, Brazil, Canada, Chile, Colombia, Ecuador, Germany, Mexico, Peru, U.S. and Uruguay off the coast of Florida during Exercise UNITAS Gold. (U.S. Coast Guard photo by Petty Officer Seth Johnson)



These issues have created frustration on the part of commercial entities whose very survival depends on containing costs, while providing very little headway in information sharing. In sum, the static of U.S. government commercial outreach drowned out the intended dialog and actions.

Need for a Forum Free from Fear of Retribution

There have been a wide range of productive outreach efforts at the federal, state, and local levels. However, the majority of these initiatives have been undertaken by agencies with a regulatory relationship with the industry. Any dialog conducted in such fora may be inherently inhibited by the regulatory basis of these relationships which in turn may prevent the acceptance by all parties of what could be mutually beneficial best practices. Leveraging best practices and establishing the trust required to move to open sharing of maritime information profits from the involvement of an honest-broker with no regulatory authority whose unbiased intent is to maximize the benefit to all parties.



CBP officers take a moment to discuss strategies for searching arriving cargo. (U.S. CBP photo by James R. Tourtellotte)

Need for Broader Industry Representation

Within U.S. government circles, a reference to the maritime industry remains too frequently a reference to its ship owners, or at the outside, its cargo interests. The industry however, is much deeper and broader in its equities. The influence and information from maritime charterers, underwriters, brokers, ship agents, private equity, mortgage holders, mariners' associations, vendors, and the seafarers themselves, presents a whole new layer of capability whose information, while routinely exchanged to the benefit of industry, remains untapped by most involved in government MDA. To not involve this layer of interests is to remain oblivious to the forces which keep global maritime trade moving forward and



(Maersk Line photo)

the wealth of maritime information they can share. These crucial maritime interests and viewpoints need to be equally involved in any changes to their industry if MDA is to benefit from their partnership.

Original Conference Intent



Based in part on the above OGMSA observations, the Global Maritime Information Sharing Symposium was created as a venue to enhance communication and maritime information sharing between government and the maritime industry. It brings together and coordinates government entities that play a role in maritime commerce in a more cohesive outreach to industry. A guiding principal is that an effective partnership with industry can only be established if the needs and concerns of industry are understood and considered. Specifically, GMISS seeks to remove inhibitors to efficient commerce through more effective

information sharing. In other words, how can we collectively enhance safety, security, and environmental management without unduly hampering commerce? More to the point, can we provide value for industry for every request for their information?

The overall objectives of GMISS are to:

- 1) Harness the growing energy for conferences and commercial outreach initiatives across the government. Specifically, identify priorities among those information sharing issues that are emerging from government-commercial relations and ensure resources are available to work them.
- 2) Create a systematic, annual cycle of coordination that ensures the identification of and engagement in government-industry information sharing issues. GMISS will be the annual meeting to assess what has been accomplished over the last year and ensure priorities are clear for the coming fiscal year.
- 3) Provide a forum for stakeholders in government and the global maritime industry to address issues and initiatives central to information sharing needs that enhance maritime situational awareness in an environment free of fear or coercion.

GMISS was specifically designed to:

- Provide synergy of efforts, husbanding of resources, and coordinated outreach across all MDA stakeholders.
- Leverage information sharing accomplishment by the Departments of Homeland Security (DHS), Transportation (DOT), Defense (DOD), Justice (DOJ), and State (DOS), industry, state and local law enforcement agencies, and maritime organizations worldwide.
- Present a single U.S. government face to the industry and help resolve their MDA concerns and issues
- Exchange best practices
- Create more trust and comfort for information sharing by industry and government

The long range (multi-year) objectives of the GMISS include:



- Coordinate the U.S. government's commercial outreach across all MDA stakeholders to ensure a more unified path for addressing industries issues/concerns and innovations.
- Implement industry/government working groups to provide a venue for maritime leaders to create problem-solving initiatives in the area of information sharing that enhance maritime security and the flow of commerce, and establish new stakeholder working relationships.
- Identify and act on opportunities that strengthen Information Sharing processes and agreements.
- Promote regional involvement.
- Engage the range of stakeholders needed to properly achieve effective maritime awareness.

Related Efforts

The Director Global Maritime and Air Intelligence Integration (GMAII) conducted a one-day “Protecting the Global Supply Chain Conference” in June 2008 as part of a series of workshops initiated by the Office of the Director of National Intelligence (ODNI) and two private sector partners: the U.S. Chamber of Commerce and the Intelligence and National Security Alliance (INSA). The purpose of the workshop was to exchange views and perspectives on issues of common interest and concern to the private sector, U.S. government and the Intelligence Community. *(please see the GMISS 2008 Summary Report, Tab 5 at www.gmsa.gov/gmiss for the full report).*

On August 20 and 21, 2008, the U.S. Maritime Administration and Naval Postgraduate School conducted their first symposium under the Maritime Information Sharing Taskforce in the ports of Los Angeles and Long Beach, coincidentally, on the same days OGMSA conducted GMISS 2008 in King’s Point, NY *(please visit www.gmsa.gov/gmiss to read the full report).*

On September 17 and 18, 2008, the Navy’s Maritime Liaison Unit – Latin America (MARLU-LATAM) conducted a Maritime Security Conference in Miami, FL that launched the unit’s industry outreach efforts.

In view of the above related efforts, in developing the GMISS 2008 Summary report *(please see Appendix H for an executive summary, or visit www.gmsa.gov/gmiss to read the full report)*, OGMSA also pooled the findings of the Global Supply Chain Workshop, MIST, the Maritime Security Conference, and other maritime awareness/information sharing research efforts and workshops. This fusion of information produced more robust and more nuanced findings than could otherwise have been generated by the discussion at GMISS 2008 alone. It also allowed the consolidated findings to serve as the foundation for development of GMISS 2009 Agenda.



Ms. Wendy Walsh, Homeland Defense and Security Coordinator at the U.S. Naval Postgraduate School, facilitates a collaborative session at MIST LA-Long Beach.

Going forward, OGMSA and GMAI partnered for their 2009 commercial outreach efforts, merging the Protecting the Global Supply Chain Conference with GMISS. The Office of the Director of National Intelligence played an integral role in planning and executing GMISS 2009.

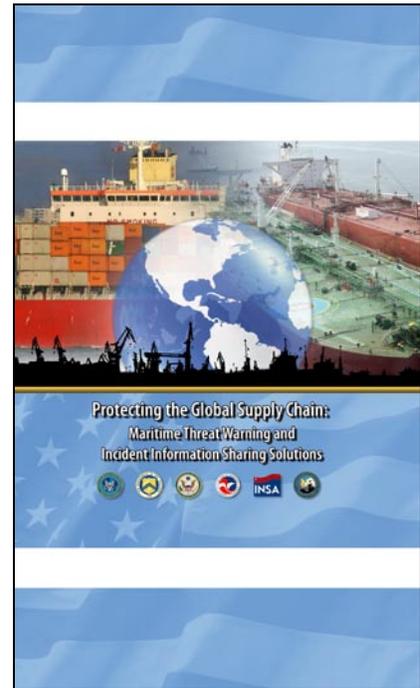
OGMSA and GMAI participated in the second MIST symposium, conducted in Seattle, WA. Prior to developing its research objectives for Puget Sound, the Naval Postgraduate School worked with OGMSA to align the desired outcomes of MIST and GMISS, and topics planned for discussion at GMISS were introduced at MIST Puget Sound. The results of MIST were integral to developing GMISS 2009.

OGMSA and MARLU-LATAM also coordinated efforts in developing the MARLU's 2009 Conference topics and GMISS 2009 to ensure each produced their own unique value and were not redundant efforts on the part of industry attendees.

OGMSA's future efforts will be coordinated with, while not hampering, these parallel conferences.

Ongoing Support

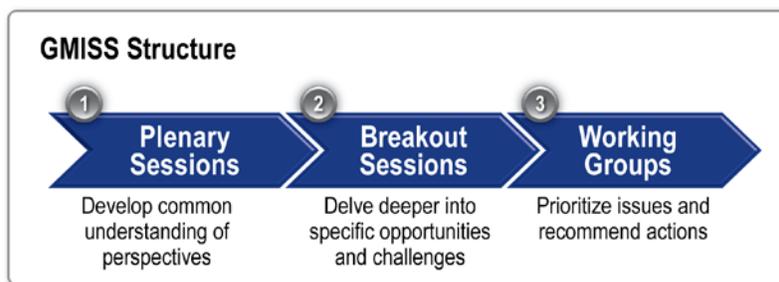
During 2009, the U.S. Maritime Administration created a complete program centered on GMISS that also supports the ongoing GMISS working groups and the Naval Postgraduate School's Maritime Information Sharing Taskforce (MIST). For the first time, seed funding has been budgeted for GMISS beginning in Fiscal Year 2010. In addition, in the wake of GMISS 2009, OGMSA received invitations from organizations worldwide to partner with them to conduct GMISS 2010 in their areas of operations. Several organizations have also contacted OGMSA to discuss their further involvement in GMISS 2010 rather than conducting their own separated conferences or symposia.





GMISS Structure

GMISS is based on an "issue-to-working-group" progression. In plenary sessions, experts from throughout the maritime domain present their perspectives to give participants a shared framework for understanding key issues. In breakout sessions, participants delve deeper into the issues that most concern them, again benefitting from the perspectives of a sampling of maritime speakers. Finally, GMISS participants form into working groups to clarify the issues, to prioritize them for action in the coming year, and to recommend possible optimal courses of action. These working groups continue to function throughout the year, ensuring the recommended actions benefit from ongoing input from the members of the maritime community with the appropriate knowledge and experience, and a stake in the outcome, and report back at the next GMISS event.



GMISS – MIST Partnership

The intent of MIST is to integrate local private sector perspectives into a national picture for the sharing of maritime threat information. Its role is to facilitate the interaction between government and the private sector and focus on delivering actionable recommendations based on private-sector input. The taskforce strives to capture the voice of the private sector maritime industry at the local/regional port level in a way that will:

- capture best practices in information sharing
- create a structure for collaborative problem solving
- convey unique local issues to national policy makers

An Interagency Approach

The MIST team is led by the Maritime Defense and Security Research Program (MDSRP) at the Naval Postgraduate School (NPS) in coordination with the U.S. Maritime Administration. Significant funding is provided by the Assistant Secretary of Defense for Homeland Defense. Conceived as a multi-agency process, MIST coordinates with a number of private and public agencies:

- The U.S. Coast Guard
- U.S. Customs & Border Protection (CBP)
- The Office of Global Maritime Situational Awareness (OGMSA)
- The Director of Global Maritime & Air Intelligence Integration (GMAII)

- The Marine Exchanges
- The Pacific Maritime Association
- Port Authorities
- Local and State Responders

The Regional Perspective

In 2008, the MIST Steering Committee created an exploratory process for facilitating the sharing of threat information. This process began with an event at the Port of Long Beach/Los Angeles that included a local workshop and a social networking web site. Continuing in Seattle in 2009, event activities expanded to include a study of local information flow and additional community building (*please see Appendix I*). Future plans include a mapping of local networks and a mechanism for consolidating national feedback. A MIST symposium in the Port of Honolulu followed GMISS in October 2009.

GMISS and MIST are now conducted in close cooperation to maintain a unified government voice for industry outreach while developing a well-rounded understanding of the industry perspective. While



The port of Seattle, Washington
(USCG photo by Petty Officer 3rd Class Tara Molle)

GMISS focuses on the national and international level, MIST reaches out at the local and regional level, ensuring local concerns and issues are understood and addressed.

To ensure that OGMSA's national/global efforts align with local/regional work efforts, both organizations look at incentives and models for information sharing and ways to streamline government requests. The MIST team probes for operational-level goals, gaps, barriers and risks in the sharing of threat information. They conduct ongoing research and meet with local port personnel for feedback on specific policies

and technologies. GMISS applies this research in ongoing working groups comprised of government and industry representatives to develop innovative strategies, and brings the industry voice to bodies that coordinate national-level maritime security policy.

This partnership ensures decisions reached at the highest levels actually make sense in day-to-day operations of ports around the country.

GMISS 2009

OGMSA coordinated every detail of GMISS 2009, including transportation, lodging, staffing, catering, and facilities, thus guaranteeing all attendees the ability to participate to the fullest extent possible in addressing substantive maritime issues during the three day conference.



Several federal agencies partnered with OGMSA to present GMISS 2009, providing valuable resources and lending their specific areas of expertise:

- The U.S. Maritime Administration (MARAD) (Department of Transportation)
- The U.S. Coast Guard (Department of Homeland Security)
- The Office of Community Oriented Policing Service (COPS) (Department of Justice)
- U.S. Fleet Forces Command (USFF)
- Naval Cooperation and Guidance for Shipping (NCAGS)
- The Naval Postgraduate School (NPS)
- The Office of the Director of National Intelligence (ODNI)
- The National Maritime Intelligence Center (NMIC)

The National Defense University in Washington, D.C. made its state-of-the-art facilities available and provided invaluable logistics, information technology, and audio-visual support without which the symposium would not have been possible.

In addition, Marine Log Magazine rolled its 2009 Combating Piracy conference into GMISS 2009. While GMISS is intended to create more effective maritime information sharing independent of any specific mission or application, no meeting between government and the maritime industry could progress far in 2009 without the topic of piracy moving repeatedly to center stage. Therefore, OGMSA positioned piracy as a useful framework for examining larger-picture challenges, successes, and opportunities during GMISS 2009.

In addition, GMISS 2009 benefited from the expertise of an amazing array of participants, which included ship owners and operators, shipping brokers and agents, vessel underwriters, maritime educators and associations, maritime information vendors, U.S. federal, state, and local officials from municipal police officers to deputy cabinet secretaries, and partners from navies, coast guards, maritime administrations and maritime operations centers around the world.

This combination of vantage points was focused in four working groups, each addressing a specific issue.

- Working Group A: Building a Global Maritime Information Exchange Grid – Is it Possible?
- Working Group B: Bridging the Understanding Gap Between Maritime Industry and Government
- Working Group C: Resolving Information Privacy, Proprietary & Classification Concerns
- Working Group D: Piracy - A Model of Co-developed, Mutually Beneficial Policies?



GMISS 2009 was held at Lincoln Hall on the National Defense University, Washington, D.C.

GMISS 2009 used a Web-based collaboration software application to ensure attendees were able to accurately and anonymously record their input during every stage of the symposium. This was particularly valuable during working groups and breakout sessions. It promoted broader participation, and enabled attendees to consistently build on points developed throughout the three-day symposium.

To ensure all participants could share their opinions, experience, and expertise without fear of reprisal, the symposium was conducted under the Chatham House Rule.¹

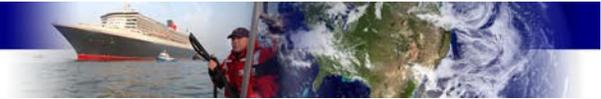
A summary of GMISS 2009 proceedings and results are presented in the following section. For more in-depth reporting of the presentations and deliberations of GMISS 2009, please see Appendix D.

NOTE: Appendix D contains summarized excerpts of individual presentations and follow-on discussions as transcribed at GMISS 2009 by OGMSA team members. It is not intended to represent all that was stated, presented or discussed nor to indicate that inclusion or exclusion of a specific concept or statement represents the speaker's position or priorities. Summarized excerpts as transcribed by OGMSA are combined with participant comments as entered by the participants during GMISS 2009, into the collaborative software. These comments may not accurately reflect what was being stated, presented or discussed.

These summary excerpts and comments are being provided for general discussion and should in no way reflect on the speakers or any individual participant.

¹ "When a meeting, or part thereof, is held under the Chatham House Rule, participants are free to use the information received, but neither the identity nor the affiliation of the speaker(s), nor that of any other participant, may be revealed" Chatham House, <http://www.chathamhouse.org.uk/about/chathamhouserule/>

² National Security Presidential Directive NSPD-41, Homeland Security Presidential Directive HSPD-13, Dec. 21, 2004



II. Summary of Proceedings

Recurring Themes

Several themes emerged during the plenary sessions, panels and breakout sessions of GMISS 2009. Because the range of presenters was selected to provide a broad cross section of the global maritime community of interest, the frequency of the introduction of these themes by both presenters and participants suggests the existence of a general consensus as to priorities among the community. For a matrix of recurring themes and their frequency of occurrence, please see Appendix F.

Coordination/Information Flow

Issues concerning the coordination of MDA were raised more often than any other (24 instances over two days). Chief among coordination concerns was a desire for clearer leadership in MDA. These ranged from the repeated industry query of “Who’s in charge of MDA?” to a desire for at least a single coordinating office in the federal government. Meanwhile, at least one government representative sought a focal point for industry on the waterfront. The Deputy Secretary of Transportation cited his experience at the state government level during 9/11 to demonstrate the value of effective coordination between federal, state, and private sector during emergencies, calling the state’s relationship with Coast



View from the bridge of a tanker entering Puget Sound (USCG photo)

Guard and industry one of the greatest strengths he was able to tap in the aftermath of 9/11. The Department of Justice, which brought a high level of expertise in information sharing to the symposium, pointed out the need for better coordination with private security providers. The Department of State sought feedback to improve OSAC coordination with industry, which was repeatedly identified by industry representatives as a model for best practices, suggesting a correlation between continuous improvement in coordination and success. A better understanding of how to improve coordination among

navies and governments was also requested. The complex web of regulations at various levels of government was introduced repeatedly. One representative of a maritime association pointed out that if government did coordinate industry information so it received all the information it desires, the government would be overwhelmed with data. The emergence of U.S. maritime spatial planning under the Obama Administration was identified as an additional layer of complexity that must be addressed and coordinated. A member of the intelligence community urged that industry be included now in government MDA architecture planning to ensure effective policies by both government and industry.

The flow of information between industry and government was the second most common topic of discussion (16 instances). Speakers from government agencies around the world stressed government's need for information from the maritime industry to accomplish a wide range of missions, such as national security and prosecution of pirates. Industry attendees made it clear that large amounts of information were available which could paint a fairly complete picture of shipping. However, government and industry participants also repeatedly raised the requirement for a better understanding of what information was needed and clarification of how to provide it to government. More than once, industry representatives explained that they don't necessarily need to know the actual intelligence information, but how to guard against or look out the possible threat, including how to best deploy resources based on current threats. A need was also expressed from inside industry for maritime companies to improve their own maritime awareness to their own benefit.

Mutual Understanding

Another related theme that emerged was the need for improved government understanding of industry and industry understanding of government. In addition to being the focus of a working group on Day Three, the topic surfaced eight times during the first two days of the symposium. There was general agreement that improved mutual understanding was essential before effective MDA could be achieved.



A container ship works with the Coast Guard during exercises aimed at preventing proliferation of weapons of mass destruction. (USCG photo by PA3 Stacey Pardini)

Classification

On ten occasions during the plenary, panel, and breakout sessions, concerns were expressed about the impact of classification on government/industry information sharing.

Most commonly, the need to reduce classification was raised. Related challenges, recommendations and successes were also offered. A perceived trend in government to move MDA programs into intelligence offices raised concerns of moving backward regarding classification.

Models for Success

Several presenters and participants initiated discussion regarding models for success in MDA (8 instances). These included:

- A matrix indicating who leads sharing of which types of data
- Need for confidence building, simple cost structure, inclusive approach
- Equal partnerships in which every participant has a say
- Start with bilateral linkages first and expand to multilateral later
- Start with closest neighbors, then expand
- Build on previous successes, such as anti-drug initiatives
- Empower participants at all levels to contribute as early as possible, following the example of the Amber Alert system.



GMISS

The symposium itself was the topic of several discussions (8 instances). For the most part, GMISS was seen as a positive development and an effective tool for improving industry/government information sharing. It was pointed out that symposium discussion must translate into actionable projects that can be tracked and accomplished. It also surfaced that GMISS objectives are being adopted by other initiatives. However, participants also pointed out flaws in the present roster of participants, identifying several organizations that must become involved for GMISS to succeed, including the International Maritime Organization (IMO), the Port Management Association of Eastern and Southern Africa (PMAESA), the Organization of American States (OAS), the European Union (EU), World Customs Organization (WCO), the South Asia Regional Port Security Cooperative (SAARPSCO), and the Maritime Liaison Office (MARLO).

Immediate Information vs. Vetted Information

During the first panel discussion on the first day, an extended discussion emerged regarding the merits of immediate information as opposed to vetted information. It became clear that both are valuable in turn. Introducing small amounts of reliable information as quickly as possible enables participants throughout the community to begin focusing on an issue and contribute both information and analysis from all levels. This was viewed by industry participants as significantly preferable to waiting until an entire vetted report could be released. The rapid expansion of awareness that rapid sharing can provide was strongly endorsed. However, the need for the information that is released by government to be trustworthy was viewed almost uniformly as essential. Industry participants also noted that they do not necessarily need to know what the intelligence is; if they are confident the guidance is trustworthy, they may only need recommendations that guide allocation of resources and an awareness of what types of threats to prepare for.

Anti-Piracy Capacity

Piracy discussions emerged throughout the symposium, as was anticipated. On several occasions outside the piracy working group the discussion centered on information sharing as it related to piracy. Three high-level U.S. government officials stated the need for more information to counter piracy (5 instances). A well-respected industry representative added that the global legal system was failing to prosecute pirates; a situation an Assistant Secretary of State indicated could be improved with better information sharing. LRIT was identified as necessary despite the universal adoption of AIS because, while AIS is often disabled to avoid detection by pirates, LRIT may allow ships under attack to continue transmitting their location of maritime security forces.

TWIC Implementation

Implementation of the TWIC program was raised on three separate occasions, with general agreement that it has been challenging. Improved sharing of information was requested insofar as TWIC could be universally recognized as valid identification, particularly across the Transportation Security Administration.

MDA Silos

On three occasions concern was expressed that some efforts to develop MDA are creating new silos rather than moving information out of silos.

Civil Liberties

The issue of civil liberties was raised three times during the plenary, panel, and breakout sessions. Notably, in all three instances, the need to safeguard civil liberties in the face of increased information sharing was brought up by federal government representatives: specifically, members of the intelligence community.



(USCG illustration by Petty Officer 1st Class NyxoLyno Cangemi)

Working Group Results

GMISS working groups met for the majority of the third day of the symposium, each addressing a unique aspect of maritime information sharing:

- Working Group A: Building a Global Maritime Information Exchange Grid; Is it Possible?
- Working Group B: Bridging the Understanding Gap between Maritime Industry and Government
- Working Group C: Resolving Information Privacy, Proprietary & Classification Concerns
- Working Group D: Piracy - A Model of Co-developed, Mutually Beneficial Policies?

After extensive deliberations, each group reported back to the entire symposium, including Points of Discovery, Actionable Items, and Ongoing Discussion Points/Recommendations.

The deliberations and results are included in Appendix D of this report in abbreviated form to ensure confidentiality for individual participants. Results are also summarized in matrices in Appendix G. Due to the specific nature of this year's working group topics, there was little overlap in findings among working groups. However, groups reported a few shared results, as summarized below.

Points of Discovery

Both Working Group B and Working Group D found that government and the maritime industry need to significantly improve mutual understanding: a concept that surfaced repeatedly throughout the symposium.



Working Groups C and D both identified a need for a single coordinating point of contact to assemble classified and non-classified information from industry and government each, and to push it out in usable form one to the other.

The need for an international maritime governance structure with clear objectives and goals emerged in both Working Group A and Working Group D.

Additional key points of discovery that arose in individual working groups concerned the need for representation of industry, government, and NGOs in the majority of maritime information sharing discussions; the need to identify specific requirements for global maritime information sharing; and identification of specific strengths and weaknesses of the global collaboration model developing to counter piracy.

Actionable Items

Two shared actionable items can be gleaned from among the four working groups.

Both Working Group A and Working Group C essentially recommended development of a network to link existing information sharing infrastructure and consolidate AIS sharing networks.

Working Groups B and D recommend evaluating existing training to improve mutual understanding, identifying gaps, and instituting the needed additional training.

Several additional actionable items were somewhat related to each other, involving:

- Global MDA governance
- Single point of contact for crises
- Specific actions to build a global maritime information sharing grid
- Building on successes
- Greater access to U.S. government policymakers
- Global marketing of maritime information sharing

Ongoing Discussion Points and Recommendations

None of the recommendations for further discussion were shared among working groups. These specifically addressed finer points of the topics each group addressed.



GMISS participants prioritize key information sharing issues in a working group. (OGMSA photo)

Shared Priorities Inferred from GMISS Working Groups

By combining shared elements from these three sets of information, OGMSA has inferred possible courses of action which may meet the shared intents and priorities of the working groups. They are as follow.

Global Grid and Governance

Identify and empower a non-governmental organization – with industry, government, and NGO representation – to provide non-binding information sharing dispute resolution between conflicting interests, and to lead:

- development of a global maritime information sharing grid
- establishment of information sharing standards
- consolidation of existing systems
- linking of existing sites and networks.

(Working Groups A, B, C and D)

Single Point of Contact for Policy

Identify a single trusted point of contact (policy vice operational) to coordinate maritime awareness/MDA development issues between the U.S. government and the maritime industry.

(Working Groups A, C, and D)

Single Point of Contact for Information Flow

Identify a single trusted point of contact (operational vice policy) to manage and coordinate ongoing information flow between the maritime industry and the U.S. government, including tip reporting, collaborative awareness and intelligence development, threat warnings, and other alerts.

(Working Groups B, C, and D)

Close Gaps in Understanding

Assess current mechanisms for developing mutual understanding between industry and government, promote these opportunities to all members of the GMCOI, and develop a plan to fill the gaps left by the existing opportunities.

(Working Groups B, C, and D)

Link Regional Centers into a Global Structure

Identify or establish regional centers to manage ongoing information flow among all stakeholders in their areas of operations; form a board comprised of these regional entities to coordinate global flow of



maritime information under an international maritime information sharing governance structure with clear objectives and goals.

(Working Groups A, C and D)

Implementation

These possible courses of action will be presented to the four ongoing GMISS working groups for consideration and refinement in order to develop a set of specific actions on which the groups can collaborate. In addition, each working group will address its own specific action items. OGMSA will provide coordination between working groups to ensure efforts complement each other.

All GMISS participants are welcome to continue their participation through the ongoing working group or groups of their choosing.



III. Ongoing Working Groups

The four working groups developed during GMISS 2009 will continue to address the issues they discussed at the symposium, and pursue their recommended courses of action. OGMSA will be using the same Web-based collaboration software employed during the symposium for some of this effort. In addition, blogs have been established for the overall GMISS effort and for each working group. All members of the Global Maritime Community of Interest are invited to join these ongoing conversations at www.GMISSonline.blogspot.com.

The four working groups will be proceeding as follows:



GMISS discussions continue at www.GMISSonline.blogspot.com

GMISS Working Group A: The Information Grid Working Group

Building a Global Maritime Information Exchange Grid

Primary Goal:

Connect maritime operations centers, fusion centers, maritime exchanges, government maritime information hubs, etc. into a global maritime information exchange grid that includes easy mariner access to provide and receive information.

Blog:

www.gmissInformationGrid.blogspot.com

Coordinator:

CAPT Ralph Nieves, USN
Chief, Data Sharing and Infrastructure Branch
Office of Global Maritime Situational Awareness
Email: Rafael.F.Nieves@uscg.mil
Voice: 202-372-3062
Fax: 202-372-3905

Actionable Items for 2009:

- Define the “as is” state of the global grid: what is useable, discoverable, accessible
 - information sharing sites
 - exercises
 - collaborations
 - laws & polices
 - best practices
- Develop a meta site of maritime information exchanges
- Create a panel to define the essential elements of information for the maritime global community
- Develop and implement a marketing plan for government / industry / NGO maritime information sharing

GMISS Working Group B: The Understanding Gap Working Group

Bridging the Understanding Gap between the Maritime Industry and Government

Primary Goal:

Enable members of industry to understand how government makes and enforces the policies that impact them, and enable members of government to understand how government policy impacts industry.

Blog:

www.gmissUnderstandingGap.blogspot.com

Coordinator:

CDR Jim Feldkamp, USN
Chief, Outreach and Coordination Branch
Office of Global Maritime Situational Awareness



Email: James.L.Feldkamp@uscg.mil

Voice: 202-372-3081

Fax: 202-372-3905

Actionable Items for 2009:

- Research the environment and evaluate training and internships currently available within the maritime industry.
 - (This training must be unclassified and examine existing inter-agency, inter-government, inter-industry training to establish a baseline for future programs.)
- Determine what is still needed.
- Establish industry participation on the Maritime Domain Awareness Stakeholder Board.
- Establish industry participate (by invitation) on Maritime Security Inter-Agency Policy Committee of the National Security Council

GMISS Working Group C: The Information Safeguards Working Group

Resolving Information Privacy, Proprietary & Classification Concerns

Primary Goal:

Identify specific barriers to information sharing based on privacy, proprietary and classification concerns, and develop a resolution that preserves civil liberties, commercial competition, and national security.

Blog:

www.gmissInformationSafeguards.blogspot.com/

Coordinator:

CAPT Dave Sanders, JAGC, USN
Legal Counsel
Office of Global Maritime Situational Awareness
Email: David.F.Sanders@uscg.mil
Voice: 202-372-3063
Fax: 202-372-3905

Actionable Items for 2009:

- Develop a pilot program utilizing existing infrastructure to disseminate information. (for example, DOD's Joint Integration of MDA program & the Maritime Information Sharing Taskforce)

- Expand the America's Waterway Watch program to encompass all users. For example, where does a commercial ship call? When the call is received, is the operator trained to deal with the information?
- Consolidate regional proliferation and global consolidation of AIS data
- Examine the use of a non-government organization to manage international governance options for global AIS sharing
- Establish a uniform U.S. government policy mandating interagency information sharing
- Develop an information sharing dispute resolution procedure
- Develop training for information sharing professionals; transform the culture of "need to know" to "need to share"

GMISS Working Group D: Operational Collaboration Model Working Group

Building on Anti-Piracy as a Model of Co-Developed, Mutually Beneficial Policies

Goal:

Develop an optimum model of operational collaboration between industry, navies, coast guards and other maritime security entities that is scalable and applicable to maritime threats and crises worldwide.

Blog:

www.gmissCollaborationModel.blogspot.com/

Coordinator:

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Chief, Plans and Policy Branch
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Actionable Items for 2009:

- Establish a single point of contact for all information flow, including reporting by industry and the dissemination of information to industry.
- Develop a single point of contact for maritime crises.
- Identify and develop the best course of action to educate government personnel that impact the maritime industry on industry commercial fundamentals.



- Build on the response models created by United Kingdom Maritime Trade Operations (UKMTO) Office and the Shared Awareness and Deconfliction (SHADE) group.
- Ensure maritime security best practices and successes are shared in a common environment to drive future development industry standards, such as the inclusion of ship defensive measure in future ship design.
- Regularly record and review lessons learned to maintain coordination between industry and government developed thus far and preserve confidence in communication.

Return to Action

The GMISS ongoing working groups are scheduled to resume operation in January 2010. Members of the Global Maritime Community of Interest who are willing to lend a hand with these efforts are invited to contact working group coordinators directly.